

air spotter association

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10072 CASELLE (TO) ITALIA

# ASA

AIR SPOTTER ASSOCIATION

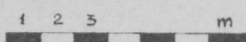
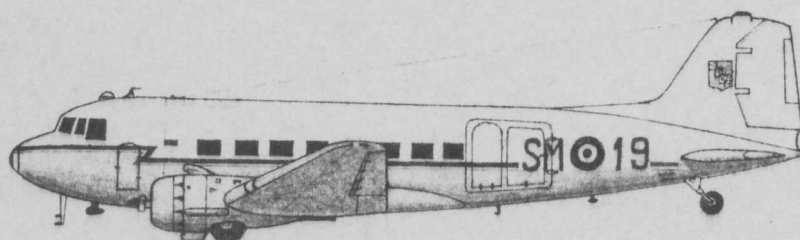
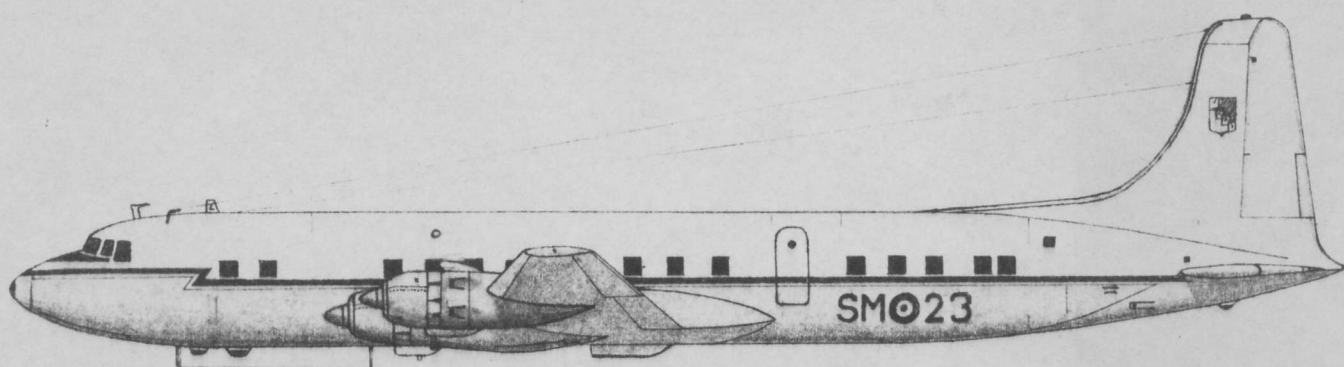
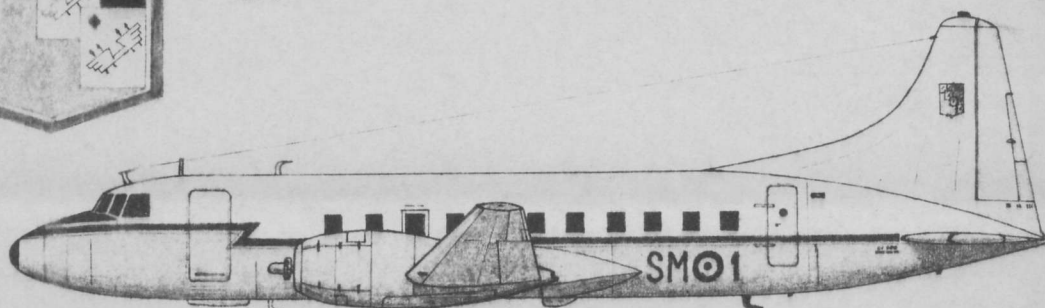
BOLLETTINO 2

GENNAIO 1972

BULLETIN 2

JANUARY 1972

GIORNALE A CIRCOLAZIONE PRIVATA  
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Cari amici,  
grazie per le decine e decine di lettere che hanno fatto seguito al numero sperimentale del Bollettino ASA. Critiche, consigli ed adesioni tanto numero se hanno dimostrato che si é colpito nel segno!

Questo Bollettino é qualcosa di estremamente nuovo, nel campo dell'editoria aeronautica italiana. Una pubblicazione agile ed aperta che già dal secondo numero introduce sensibili novità, senza allontanarsi peraltro da quello stile dilettantistico che le permette di avvicinarsi più incisivamente agli interessi specifici di coloro che lo fanno e poi lo leggono, cioè gli "spotter". Questo non é un discorso esclusivistico ma balza agli occhi che proprio nei paesi dove la mentalità aeronautica é più sviluppata (con i positivi effetti che non é mio compito elencare) più folte sono le schiere che invadono gli aeroporti armate di taccuini, binocoli e macchine fotografiche.

L'introduzione della stampa in offset, l'aumento delle pagine, insieme al continuo giungere di validi suggerimenti, correzioni e nuove notizie ha non poco ritardato l'uscita di questo secondo numero. Inoltre, mentre la redazione era già così impegnata, veniva formata l'ASA-Toscana (pag.9), si sviluppava positivamente l'iniziativa "Visita ad una portaerei dell'US Navy" (pag.3) e si ponevano solide basi alla partecipazione dell'ASA al 5° Salone dell'Aeronautica e dello Spazio di Torino.

Le idee dunque non mancano, non resta che proseguire su questa linea improntata alla massima collaborazione fra tutti noi. La redazione attende dunque notizie, foto ed articoli da pubblicare sul prossimo Bollettino.

Il Grande Spotter

Dear friends,

thank you for your letters and subscriptions followed to the experimental ASA Bulletin. I remember you that a this type publication is lone in Italy and that it is made principally to increase the number of aviation enthusiasts in this country, with your support; in the same time Italian spotters at last will give you a little more knowledge of the yet so unknown aviation world in our country.

The delay of this second number is been caused by the introduction of the offst print system and by several editing problems. In the same time ASA staff was engaged with other initiatives: the foundation of ASA-Toscana (page 9), a visit to a US Navy aircraft carrier and the participation of ASA to the 5° Salone dell'Aeronautica e dello Spazio in Torino.

The Great Spotter



This is the first of a series of reports in which the present situation and the development of the Italian national carrier will be treated. It is devoted to the Douglas DC-8 which had and has so a great role in the growth of the airline.

The first Alitalia jet plane, a DC-8/43 landed at Roma Ciampino in April 1960 and on June 2, I-DIWA flew to New York Idlewild (later J.F. Kennedy) displaying what would have been for many years the symbol of the company: a full tricolour on the fin. Acquired to replace the DC-7C on the intercontinental network, the new bird gave very good results on this classical route. At the end of 1961 Alitalia reached 7 deliveries and the last (and 15th) Conway engined DC-8 was delivered in 1965.

Two years later arrived the first improved -62 series, which enabled Alitalia to fly non-stop Roma-Rio de Janeiro or so with full payload. In the meantime, the increasing long-range freight transport involved the Italian flag carrier to order two DC-8/62F to be employed mainly on the North Atlantic.

The last two DC-8/62s (I-DIWH and I-DIWX) arrived at Fiumicino in immaculate white livery to receive directly at the maintenance base the new colours and the impressive "Big A" on the fin, the brand of Alitalia in the '70ties.

With the introduction of 747s on the North Atlantic, the DC-8/43s have been transferred on the routes towards the African countries and domestic too (Roma to Milano-Linate, Palermo and Catania). Two will soon be operated by S.A.M. that at last will return on the Atlantic.

Besides the regular "transit inspections" and more accurate "base inspections", the Alitalia's DC-8s receive the "zero" and "D" maintenance operations, carried out every 150 and 500 flight hours respectively. Two major overhauls, "2E" and "3E" are performed annually and biennially and take a number of weeks.

Registr.	Series	c/n	Delivery	Fleet name and notes
I-DIWA	43	45598	28/4/60	Amerigo Vespucci
I-DIWB	43	45625	31/5/61	Antonio Pigafetta
I-DIWC	62F	45960	19/4/68	Titano - first flight 26/3/68
I-DIWD	43	45631	24/3/62	- crashed near Junnar (India) 6/7/62.
I-DIWE	43	45599	5/7/60	Cristoforo Colombo - to Zambia AW as 9J-ABR.
I-DIWF	43	45630	28/2/62	Antoniotto Usodimare - crashed near Cuirone di Vergiate on 2/8/68.
I-DIWG	43	45660	14/4/64	Luca Tarigo
I-DIWH	62M	46132	24/7/70	Giovanni Pierluigi da Palestrina
I-DIWI	43	45600	27/7/60	Giovanni da Verrazzano
I-DIWJ	62	45986	26/7/68	Antonio Vivaldi - fuselage N°379
I-DIWK	62	46082	15/4/69	Giacomo Puccini
I-DIWL	43	45682	21/3/65	Nicolò Zeno - struck by a rocket in Middle East returned in service on 28/6/70.
I-DIWM	43	45755	15/4/65	Ugolino Vivaldi
I-DIWN	62	45909	17/11/67	Giuseppe Verdi - f/f 12/10/67 del. Long Beach-Roma non stop 11 hours and 31!
I-DIWO	43	45601	2/11/60	Marco Polo
I-DIWP	43	45636	3/11/60	Alvise Cà da Mosto
I-DIWQ	62F	45961	30/6/68	Ciclope - fuselage N°361; ex N8964U
I-DIWR	43	45637	1/2/62	Niccoloso da Recco - chartered by Zambia AW 1969.
I-DIWS	43	45665	12/12/63	Leone Pancaldo - painted with Vatican colours for Pope's travel in Holy Land (photo 6, Maranta).

SPOT - NEWS  
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I-DIWT	43	45666	14/4/64	Emanuele Pessagno
I-DIWU	43	45624	28/4/61	Giovanni Caboto
I-DIUV	62	45910	9/12/67	Gioacchino Rossini
I-DIWW	62M	46098	10/2/70	Arcangelo Corelli
I-DIWX	62M	46142	12/3/71	Luigi Cherubini
I-DIYW	62	46027	25/2/69	Vincenzo Bellini
I-DIWZ	62	46026	30/4/69	Gaetano Donizetti - crashed at NY Kennedy airport on 15/9/70.

It's interesting to note that Alitalia leased, for some months between '68 and '69, DC-8/54CF N108RD c/n45663 from Airlift Int.l (photo 7 at Fiumicino, Maranta).

#### EMENDMENTS AND CORRECTIONS to N°1

##### Caproni Collection (from G. Gambarini, M. Rossi and G. Borello)

Add CA-53 Triplanino and an Aviatik A-2; I-LANC is a S.A.I. Ambrosini 2S; I-BIZZ Ca-100 (MM56271) with FIAT A-50 engine; I-MOLG doesn't belong to the museum, in fact it is a... F-15B Picchio (c/n21); the aircraft reported as SVA is an Ansaldo A-1 "Balilla", designed by Ing. Brezzi, it derives from the SVA family and differs mainly for the interplane bracing form and for the bulges housing the machine guns.

##### The airline of the Alps (from G. Gambarini)

Porters I-ONDI and I-SORE stored at Genève Cointrin and damaged by a storm on 14/8/1969; Skyvan I-TORE damaged at Venezia on 6/3/67 and scrapped; Skyvan G-ASCN (prototype) was used jointly by Aeralpi and Short for test flights; I-CE SA was returned to Short as G-AXCS and dismantled; DHC-6 I-CLAI c/n30 crashed at Colle Visentin (Vittorio Veneto) on 11/3/67. All the "Twin Otter"s were sold to Mackey aircraft broker: I-ANTE to N970JM, I-CHAN to N935JM, I-FALO to N955JM, I-NUVO to N995JM, I-TOFA to N965JM. Other sources report that they were sold to Fairfield Avn. then to FB Ayer and Mackey or first to FB Ayer and after some sold to Fairfield and remaining to Mackey!!! Aeralpi officially ceased operations on July 2nd 1968.

##### Lone flight airline (from L. Bertolo)

During 1969 and 1970 Turavia was operating a little fleet of light aircraft for commuter flights and other tasks :

I-INVA S-205/20R c/n 375; I-INVE P-66B c/n 52; I-INVI P-66B c/n 53.

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**ATTENZIONE!!!!ATTENZIONE!!!**

**UNA FANTASTICA INIZIATIVA DELL'ASA PER I PROPRI MEMBRI**  
(in collaborazione con le Ambasciate U.S.A. di Torino e Roma)

#### VISITA ALLA PORTAEREI CVA-62 "INDEPENDENCE"

A Genova nel mese di Aprile. Tutti i membri interessati DEVONO SCRIVERE IMMEDIATAMENTE in redazione dando la propria adesione formale all'iniziativa. Riceveranno subito le informazioni per il ritrovo a Genova.

ADERITE SUBITO ! il gruppo dovrà essere di 40 persone ed Aprile é vicino.

....un motivo in più per associarsi



## SPOTT - NEWS

### ALL READERS CAN SUPPLY NEWS AND PHOTOGRAPHIES

The last four Alitalia "Viscount"s have been sold; I-LIRG c/n 248 has been ferried to the "Francesco De Pinedo" Institute at Roma, for ground instruction (there is also G-46/1B I-AEHP), while I-LITS c/n 119, I-LOTT c/n 330, I-LIFT c/n 326 have been bought by Aerolineas TAO (Taxi Aero Opita) and scrapped at Ciampino. Sometimes another ex Alitalia "Viscount", I-LILI, now HK-1061 come from Colombia to load spares recovered from the three planes.

Aerolineas TAO have planned to buy some Alitalia "Caravelle"s when these will be withdrawn from service.

ITAVIA DC-9s actually in service are two. The first is I-TIGA c/n 45728 an -11 series, originally delivered on 19/12/65 to Bonanza Airlines (now Air West) as N945L; it is arrived in Italy on 9/10/71 and its first service was on 29/10/71. This aircraft suffered an accident while running during training tests at Roma Ciampino on 4/1/72 and nosewheel collapsed and returned in service two weeks later. (for I-TIGA see photo 12 taken at Bologna-Borgo Panigale on 27/10/71 by M. Tampellini). The second is I-TIGE c/n 45717 a -15 series coming from Hawaiian Airlines that was operating it from 12/3/66 as N901H. It was delivered to Itavia in November last year and received the Italian registration on 15/1/71 at last. Itavia uses its DC-9 fleet (two more will come) mainly on the scheduled network with a 90 seat layout.

The Aermacchi-Aerfer AM-3C is the only foreign aircraft to take part to the final evaluation for the PAVE COIN (anti guerrilla and forward air control) program. The prototype I-AEAM made a series of test flights with various armament combinations at Englin AFB, Florida, carrying U.S. marks.

The second batch of 27 G-91Ts for German Luftwaffe is being assembled at the Aeritalia (FIAT) plants in Torino and Caselle. Ground tests have to start soon. Started on January, with four aircraft, the deliveries of AB-206 A-1s to the air detachment of Brigata Alpina Taurinense based at Venaria airfield near Torino. They will replace progressively Piper L-21Bs, that will be transferred to the air detachment of Brigata Alpina Cremonense, based at Venaria too and expecting the definitive choice between AM-3C and SM-1019 to be equipped in a more modern manner.

The Agusta version of "Jetranger" for the Esercito (Army) is basically similar to the U.S. Army Bell OH-58A "Kiowa".

### Italian prototypes

-ITALAIR SF-20 "Pegaso" I-GEAV made first flight on 21/10/71 at Milano-Bresso, the second exemplar is near completion. The new Italian firm has planned to build definitive plants near the Valbrembo airfield.

-Agusta A-109 "Hirundo" flew for the first time at Milano-Malpensa on 4/8/1971.

-While new Partenavia plants are under construction near Napoli-Capodichino airport, second "Victor" I-GAUS flies with ALIFOTO based at Torino-Aeritalia, equipped with a Kg. s 125 camera mounted internally and accommodation for four people. It was delivered on 25/1/72. Third P-68 I-VICT is actually in California, on evaluation by Aero Commander Div. of North American.

Cherokee Six I-JUMP c/n 32-7140074 (deliv. reg. N1983T) has been bought by the Para Club of Torino to replace old "Forwarder" I-FULF. It is provided with a large door on the port side (see photo 11 taken at Torino/Aeritalia by L. Bertolo).

The P-136, like all the post war Piaggio types, was designed by Ing. Faraboschi and Prof. Casiraghi. It appeared for the first time to the public at the 1947 Paris Show. Particular features of the P-136 are its gull wings and push engines, which are blended to a low-drag hull and tail surfaces. Despite its clean line, the hull has a roomy cabin with accommodation for five people; the extensive glazing gives a very good all-round view.

The P-136 was originally fitted with two 215 hp. Franklin engines, and the first batch for the Aeronautica Militare were of this type, used mainly in low-coast rescue service. In 1951 the prototype was re-engined with Lycoming GO-435s and became the P-136L; it was this plane in which marchesa Carina Negrone set up an international amphibian distance record from Ghedi (Brescia) to Luxos (Egypt) 2986 kilometers.

But success came when Mr. F.J. Trecker, an American machine-tool magnate of Milwaukee, bought one, liked it and set out to sell and assemble it in the U.S.A., calling it the "Royal Gull". This version was the P-136L-1, with 270 hp. Lycoming GO-480 engines, it sold very well and was ordered by the AMI to replace its lower-powered Franklin aircraft.

Ten years after its first flight, the P-136L-2 with supercharged Lycoming GOS-480 came out from the Piaggio plants, with a bigger payload and higher performances. This became the "Royal Gull" Super 200 in the U.S.A. Final deliveries of this nice amphibian were made in 1957, the last being owned by Mr. A. Onassis.

#### TECHNICAL DATA

Type	Length (metres)	Span	Empty weight	Max. weight	Max. speed sea level	Range
P-136	10,80	13,48	Kg. 1344	Kg. 1832	Kmh. 254	Km. 1240
P-136L	10,80	13,53	Kg. 1351	Kg. 1904	Kmh. 283	Km. 1760
P-136L-1	10,80	13,53	Kg. 1444	Kg. 1954	Kmh. 296	Km. 1600
P-136L-2	10,80	13,53	Kg. 1528	Kg. 2176	Kmh. 341	Km. 1500

#### PRODUCTION LIST

c/n	Type	Year	Deliv. reg.	Notes
101	P-136	1949	I-FIMA	Piaggio, Onassis, 1960 Alisud, Williamson, Campbell, w/o Lee on Solent 14/9/63
102	P-136	1949	SU-AHM	King Faruk.
103	P-136	1949	I-PIAG	Piaggio, 1951 to P-136L, 1960 Alisud, scrapped.
104	P-136	1950	MM56702	AMI, I-NEVE.
105	P-136	1950	MM56703	AMI
106	P-136	1950	MM56704	AMI
107	P-136	1950	MM56705	AMI
108	P-136	1950	MM56706	AMI
109	P-136	1951	MM56707	AMI, to P-136F (?), I-TEMA Ae.C. Bologna (photo 3 by L.Perinetti at Bologna).
110	P-136	1951	MM56708	AMI
111	P-136	1951	MM56709	AMI
112	P-136	1951	MM56710	AMI
113	P-136	1951	MM56711	AMI
114	P-136	1952	MM56712	AMI
115	P-136	1952	MM56713	AMI
116	P-136	1952	MM56714	AMI
117	P-136	1952	MM56715	AMI
118	P-136	1952	MM56716	AMI



(continuation)

<u>c/n</u>	<u>Type</u>	<u>Year</u>	<u>Delivery reg.</u>	<u>Notes</u>
194	P-136L	1954	N222A	Trecker Co, Kearney & Trecker, Duncan Beechcraft.
195	P-136L	1955	G-AOFN	Lambert Trust Co, Onassis, 1958 Piaggio to P-136L-1
196	P-136L-1	1955	N219A	Trecker Aircraft Co., Big Brother Aircraft, New London Airport.
197	P-136L-1	1955	N220A	TAC, Eastland Tester Ltd., CF-IZA Geolaire, crash on 17/6/1957 near Belwood.
198	P-136L-1	1956	N221A	TAC, Commodore Avn., Rainbow Center Inc., 1961 Mr. DiNapoli (San Jose Calif.), Edward McCommon & Partners Equipments Co.
199	P-136L-1	1956	CF-ILU	TAC, Timmins Avn. Inc., Eastern Provincial A/w, sunk and destroyed on 31/7/1959 at Newfoundland.
200	P-136L-1	1956	N40022	TAC, DeVlieg Machine Co., Carl Gruninger, 1967 Paul Smith.
201	P-136L-1	1956	CF-IZB	TAC, D.A. McKinzie, Wings Ltd., March '57 Leasair.
202	P-136L-1	1956	MM80076	AMI, 26/6/61 I-BEPE, Alisud, actually stored at Parma airfield.
203	P-136L-1	1956	MM80077	AMI, I-ALBG Palermo.
204	P-136L-1	1956	MM80078	AMI code 84-16.
205	P-136L-1	1956	MM80079	AMI, 7/6/58 I-GIOJ, w/o 1962.
206	P-136L-1	1956	N40023	TAC, Bud & Dennis Branham, 1967 N160E CAVU Co.
207	P-136L-1	1956	CF-IZG	TAC, March 1957 Dept. of lands & forests, N100W William Palmer III.
208	P-136L-1	1956	SE-CDE	Degens Nyheter (Stockholm). Photo 1 from Perinetti.
209	P-136L-2	1956	N220A	TAC, Pan Air Corp.
210	P-136L-1	1956	MM80080	AMI, from 1961 to 1964 I-AELO MDA, 1964/5 Gruppo Aeroturistico Ligure-Genova, 1965 Lester V. Halm as N16160.
211	P-136L-1	1956	MM80081	AMI, I-GULL MDA, Piaggio from 1961.
212	P-136L-1	1956	MM80082	AMI, 1958-1963 I-SASO MDA, from 1964 I-FRLV Ae.C. Varese. Photo 2 from L. Perinetti collection.
213	P-136L-1	1956	MM80083	AMI, w/o

(to be continued)

#### News from the Belgian Air Force

a correspondence from M. Depaepe

- CM-170 : We received some more Fouga Magister. Four come from Germany: MT-46, MT-47, MT-48, MT-49 respectively c/n 145, 203, 204 and 224. A few other come from France. I don't know yet the c/ns but they have received serials of formerly crashed aircraft.
- SV-4 bis : the very last were sold. V16 and V22 to United Kingdom. V41 as OO-LUK. V15, 18, 20, 27, 29, 30, 36, 38, 61 sold to France, Germany and one remains in Belgium.
- T-33 : there are rumours that those loaned to Netherlands came back and that a few are back in Belgian service.

...PUBLICITY HERE TOO AGAIN !!!!!

ASA is the Italian distributor for :

"Aircraft Markings '71"

printed by JP Editions, Zurich.

The book contains 7000 registrations of aircraft covering all the world airlines and including several pages of pictures.

Price for ASA-members : Italy £It.2000 abroad £It.2200 (postal fee comprised)

Price for others : Italy £It.2900 (postal fee comprised)

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Mr. Gianni Siccardi (Segretario), C/o ASA Via Caldano 4, 10072 Caselle

## 306° GRUPPO DA TRASPORTO - REPARTO VOLO STATO MAGGIORE

by Roberto Reggianini and Gianni Siccardi

The Reparto Volo Stato Maggiore (High Command Flight Unit) was established on 15/4/1946 at Roma/Ciampino airport. Initially using aircraft of various transport groups, in a second time it was enclosed to the flight unit of the Zona aerea Roma (code ZR-) later Regione aerea Roma (code RR-).

Now R.V.S.M. is conglobate with 306° Gruppo Trasporti based at Latina and operates a large fleet of transport aircraft coming mainly from L.A.I. (Linee Aeree Italiane) and Alitalia, on lease to A.M.I. after the Withdrawal from civil service. For logistic reasons a part of this fleet is based at Roma Ciampino.

The DC-6s and CV-440s (DC-3s are to be withdrawn from service in a short time) are used to carry diplomatic missions, both through and abroad, civil and military VIPs and for the travels organized by the A.M.I.

CV-440 SM-1 (Italian "Air Force One") is used exclusively as presidential and VIP transport. For this last duty two DC-9s have been ordered.

The minor fleet comprises a pair of P-166M and one or two Helio "Courier" as utility and liaison aircraft, while several PD-808s twin-jets are being delivered. Six are already in serviced without code number.

### AIRCRAFT ACTUALLY USED BY 306° GT/SVSM

CODE	MM	c/n	type	remarks
SM-1	61833	442	VC-131	MDA 23/10/57; call sign I-MIDA (photo 8 Caselle 10/9/68 by L.Perinetti)
SM-4	61898	392	VC-131	was LAI I-DOVA.
SM-5	61615	44251	C-118	was Alitalia I-DIMA; SAM.
SM-6	61900	43152	C-118	was LAI I-LIKE; Alitalia I-DIMC; del. to AMI 1963.
SM-7			C-47	
SM-8	61923	43217	C-118	was LAI I-LOVE; Alitalia I-DIMT.
SM-9	61775		C-47	
SM-10	61778		C-47	was SM-13; long-nose.
SM-11	61817	7325	C-47	was LAI I-EBRO; 42-15530 USAAF.
SM-16	61832		C-47	
SM-18	61811		P-166M	camouflaged.
SM-19	61777	9910	C-47	was PH-TBK.
SM-20	61826	4380	C-47	was CR-44; MDA I-RIBE; LAI I-LIRA; 41-18342 USAAF.
SM-21	61899	407	VC-131	was SM-2; LAI I-DOVE.
SM-22	61901	450	VC-131	was SM-5; Coca-Cola Company.
SM-23	61964	44253	C-118	was Alitalia I-DIMI; SAM
SM-25			C-47	
SM-26	61922		C-118	
SM-29	61768	11760	C-47	was SM-23; 42-68833 USAAF
SM-30	61779		C-47	
SM-31	61818		C-47	
SM-32	61894		C-47	
SM-33	61825		C-47	
	61948		PD-808M	(photo 10 taken at Bologna by M.Tampellini).
	61949			
	61950			
	61951			
	61952			
	61953			



## REQUIEM FOR THE C-119J

by F. Federighi

The Fairchild C-119Js of the 50° Gruppo, 46th Aerobrigata Trasporti Medi are being withdrawn from service; in fact the first batch of four aircraft has been scrapped at the SIAI factories of Vergiate airfield. They are:

46-55 MM51-8144 ; 46-59 MM52-5949 ; 46-60 MM52-5851 ; 46-62 MM51-8158.

46-68 MM53-8103 is due to be scrapped in a short time.

All the remaining C-119Js will follow the same fate and the 50° Gruppo will be equipped entirely with Lockheed C-130Hs.

The first 3 "Hercules"es will be flown from United States to Pisa by Italian crews in early March; they will start operations in June, followed by 4 more during the year while the other 7 will arrive in 1973.

## R.A.F. OPERATIONS AT PISA

by Domenico Farina

In the ambit of the NATO inter-allied drill program, combined RAF/AMI operations have been held for the second time in November 1971.

British aircraft have joined Pisa "San Giusto" airport on November 7th and 8th and the following day, at 5 a.m., the C-130Ks have started to take-off towards Sardegna where they had to drop Italian paratroops.

Some minutes after take-off, the Hercules XV216 had a tragical accident and crashed into the sea near the Meloria's shoals, about 10 Kilometers from the coast. There were no survivors on board, 6 British crews and 46 paratroops of Brigata Folgore were killed.

All flights were immediately cancelled and restarted on November 13 when an o-ther C-130K, XV217, came to replace the lost one.

The operations ended on November 19th when Belfast XS365 "Hector" brought back to Britain handling material and spares.

During the operations HS-748 Queen's Flight, "Dominie", 2 "Andover"s and 2 "Puma"s have been seen.

Lockheed C-130Ks which took part to the operations were:

XV176 XV179 XV190 XV198 XV201 XV202 XV206 XV209 XV216 XV217 XV290 XV297 XV302.

A S A - T O S C A N A is born.

On 27/12/71 was founded ASA-Toscana with same aim and statute of the original ASA in Torino, that obviously becomes ASA-Piemonte (and ASA general head office).

In the first session Roberto Farina was elected President of ASA-Toscana and Domenico Canali Secretary. Other promoters were Antonio Mancino and Federigo Federighi. ASA-Toscana now collects the activities together of a numerous group of en-thusiasts in the zone of Pisa and Firenze.

The two previous reports come from this very active group, an exemple for other scattered spotters in Italy.

....have a good work ASA-Toscana's friends!!!

..last minute amendment.

contrarily to the report in ASA Bulletin N°1 (Spott-news) Jet Commander I-ARNT is owned by DiPalo Society of Torino. John Deere-FIAT Trattori company at pre-sent is still using aircraft of the FIAT fleet waiting for its own one.

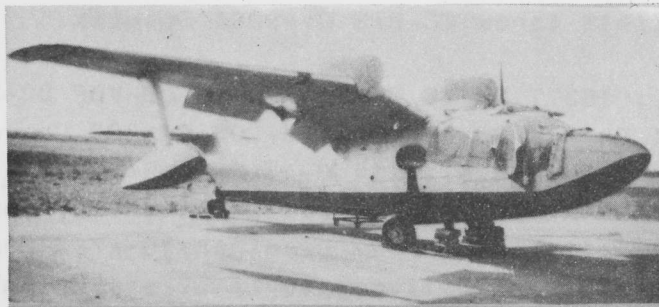
Photo 4 : Super Guppy F-BGTV, see Caselle movements at page 10.



1



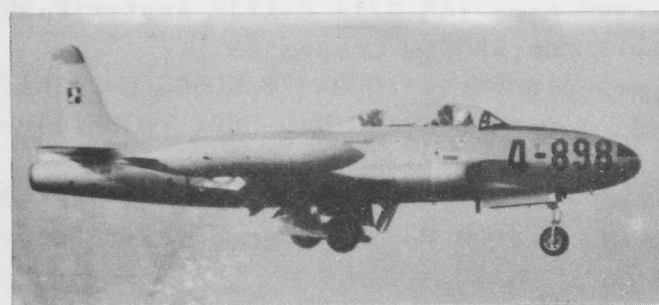
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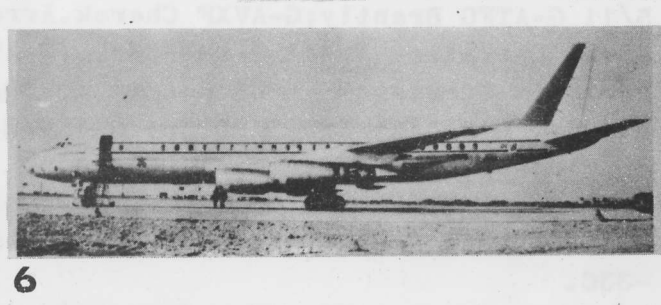
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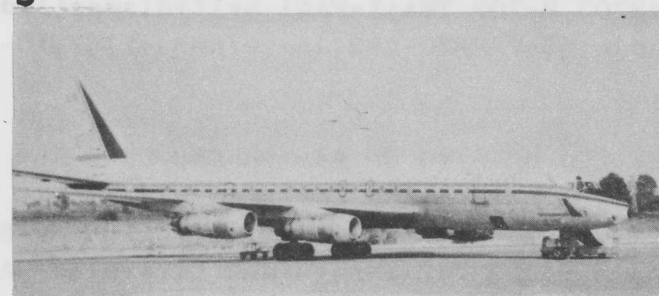
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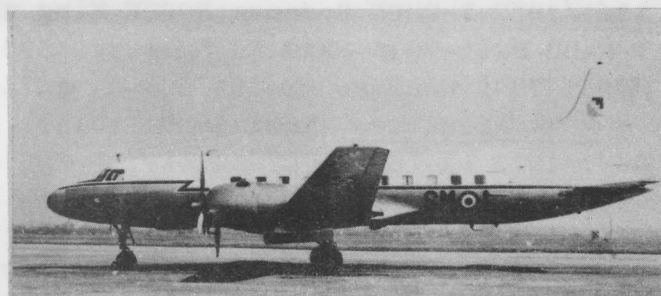
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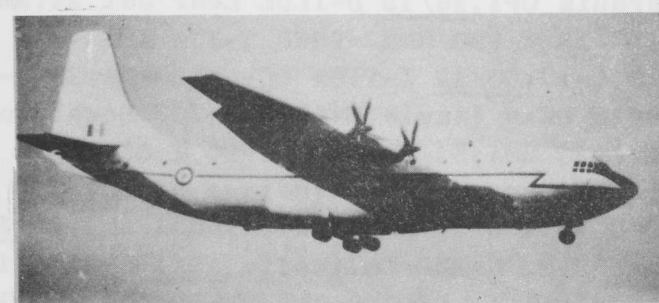
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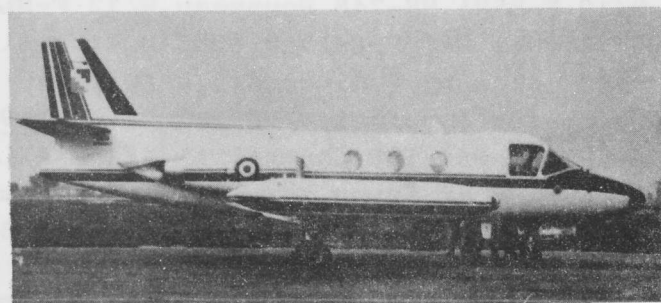
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8



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10



11



12



## AIRPORT MOVEMENTS

### AMSTERDAM Schiphol (Dick Gortzak)

12/12 JA8033 DC-8/62 JAL; G-ASZS GY-80; G-AWZK Trid.3 BEA; G-AXRO Pa-30; D-ADIM DC-8/33 Atlantis; PH-APU Pa-30; N7907C DC-8/63 Capitol (new livery, white C on blue tail); PH-USH Cessna 172; G-AVTW B-707 B.Cal.; OY-APF F-27 Maersk. 19/12 D-AHLB F-28 Aviation; JA8016 DC-8/53 JAL; HB-DVZ Mooney; F-BRSC Beech Duke; G-AWZM Trid.3 BEA; 18451 CH-54 A Skycrane USArmy (819th Sqd. AS-49); YR-IME Il-18 Tarom; EC-BTE Cv-990A Spantax.

### BIRMINGHAM (Geoff Perry)

9/11 N228W Turbo Commander; D-IKOR King Air; 16377 UH-1D USArmy. 11/11 OO-VDF DC-3 Delta; OO-CTK DC-6B Pomair; N711Z Jetstar. 16/11 G-ALBM Dove; G-AVCI B-206; G-AZDJ Cherokee; G-ARFZ Dove. 19/11 G-APWH Herald BIA; G-ARYU Cessna 320. 20/11 G-AYLP Yankee. 21/11 G71 (84001) C-130E R.Swed. AF. 22/11 G-APPX Viscount Air International. 24/11 4X-FAV Norat. Israel AF; G-AXZC Cherokee; G-AOLK Prentice. 25/11 LN-SUL F-27 Braathens; G-AZFR Cessna 401; XV268 Beaver. 27/11 HK-1267, HK-1347 Viscounts on delivery to LA Urraca Colombia.

### COVENTRY Baginton (Geoff Perry)

5/11 G-ATFG Brantly; G-AVXF Cherok. Arrow; G-ATBH Aero 145. 7/11 G-AIJM Auster; F-BRZK Jodel Chevalier. 12/11 G-AMZN Dove; G-ATVE Bell 206; N8096Q Cessna 421. Hunter's fuselages in HS hangar on 30/10 included : XF441/G-9-377; XE585/G-9-354; WT804/7789M; G-9-342; XF323/8003M; XF459/G-9-355; G-9-356; WV386/G-9-368; 7950M/G-9-341.

### MILANO Linate (Claudio Ventura)

6/1/1972 I-CART Lear Jet 24D c/n231 ex HB-VBU Fratelli Fabbri Ed.; I-JAKA Yak-40 c/n 9020409; N6695A Gran Commander; OD-AFP B-720 MEA; F-BPUF F-27 A. France; I-RAUL Beech E-33C.

### ROMA Ciampino (Luciano Bertolo)

16/1/1972 I-BMBN Bonanza; N853K King Air; N18703 B-707 TWA; I-FANI Svit (dismantled); I-FABO Svit-70; I-SNAM FJ Falcon; N6595L Navajo; 15-7/MM50-182, 15-10/MM51-7157, 15-12/MM51-7252 Grumman UH-16A S.A.R. AMI.

### ROMA Fiumicino (Luciano Bertolo)

16/1/1972 LX-LGW B-707 Luxair; CF-CPG and CF-CPJ DC-8/43 CP Air; PH-DNO DC-9 KLM new livery; YV-C-VID DC-8/53 VIASA; F-BHSQ B-707 A. France; OD-AFF Coronado MEA; SX-DBC 707 Olympic; N788TW and N767TW B-707s TWA; N797PA and N446PA B-707 Pan Am; PH-DCM DC-8/53 KLM; TS-ITU Carav. Tunis Air; N738PA 747 Pan Am; N93102 747 TWA; YU-AHD Caravelle JAT.

### TORINO Caselle (Marco Martilla)

8/12 F-BSRA HS-748 Rousseau. 9/12 F-BLLU Marquis OPI. 16/12 D-ILDE Lear Jet 24; XM469 Britannia RAF "Regulus"; N253ZM King Air 90; 27/12 4-898 MM52-9898 T-33A AMI (see photo to 5 taken by G. Siccardi); G-AWWY BAC 1-11 B.Cal.; 30/12 I-TIGA DC-9 Itavia; SSSR-11108 An-12B Aeroflot c/n01347803. 3/1/1972 N901H DC-9 Itavia; G-AYVG B-707 Donaldson; G-APMB Comet IVB Channel AL. 4/1 G-AXLN BAC 1-11 BMA; 51-85 and 51-86 T-33A AMI. 7/1 F-BTGV Super Guppy Aeromaritime (from Istres to carry rear fuselage of the 2nd Mercure); MM6769 and MM6770 F-104S; MM6566 F-104G. 11/1 9-32 MM51-9253 T-33A AMI. 16/1 I-FFSC Cessna F-172. 17/1 I-TIGE DC-9 Itavia; 50+82 C-160 Luftwaffe. 19/1 SSSR-11118 An-12B Aeroflot. 5/1 SSSR-11107 An-12B Aeroflot. 22/1 SA-40 G-91T MM6340 n.c.70; G-AXJM BAC 1-11 BCal. 23/1 CR-45 C-47 MM51776; D-AMIE BAC 1-11 Germanair; 27/1 I-DIWL/WS DC-8 Alitalia; G-AWZA Trid.3 BEA; G-AXFE King Air; F-BSRO King Air; F-BSTR F.J. Falcon; I-ATIW DC-9 ATI; HB-GDZ Duke. 29/1 G-AWYW BAC I-II BCal. 31/1 36-66 T-33A AMI. 6/2 D-AMAT BAC 1-11 Germanair; I-DEME B-747 Alitalia (training). 11/2 I-DEMA B-747 Alitalia (train.) I-SARU Queen Air Aertirrena. 12/2 G-AWYT BAC 1-11 BCal. 15/2 SSSR-12991 An-12B Aerof. I-TALE SA-318C c/n2025; F-BTCX Navajo; RM-38 MM61974 S-208M AMI; SSSR-11105 An-12B Aerof. 01347801; 3-20/3-21 RF-104Gs; G-91R MM6253 n.c.19; G-91Ts MM6353 and MM6355; G-91Y MM6469 n.c.2031. 20/2 G-AVSO Aztec; OH-KDB DC-6B Kar Air.



servizio stampa e propaganda del

**salone internazionale dell'aeronautica e dello spazio - torino**

5th INTERNATIONAL AEROSPACE SHOW

IN TURIN FROM JUNE 1 TO 11, 1972

The 5th return of the International Aerospace Show will be held from June 1 to 11 1972 at the Fair quarters built at the international Airport Torino-Caselle.

The very modern premises - open in the occasion of the 4th Aerospace Show in June 1970 - are divided into 8 pavilions built with modern steel structures and containing a total of about 10.000 sq.m. display covered area in all.

Other six or seven thousand covered area comprehend offices, châteaux for exhibitors, services (restaurants, bars, conference rooms), and boxes for specialized press etc.

At the sides of pavilions, about 300.000 sq.m. open area will be occupied by displays of aeroplanes, helicopters, equipment for airport activities as well as by large parking areas for both exhibitors and visitors.

In the course of the last two years the organizers of the Show have done their best in order to improve the services, particularly as regards the food and restaurant section. Moreover, a new power station which will deliver 1000 kwh is in an advanced construction stage. This will permit to improve the illumination as well as to integrate the delivery of the motive power to the pavilions and the open areas.

The 5th International Aerospace Show, Turin from 1 to 11 June next will be divided into three specialized sections:

aeronautics: aircraft and their parts - motors and their parts - hydraulic and mechanical equipment - air electronics - meteorology - studies and experiences - air trade - aerial works: school, photography, promotion, agriculture, tourism, aeroclubs.

space: missiles, rockets and their components - satellites - airships - motors and their parts - ground and air equipment - electronics - models - studies and experiences - applications.

airport equipment: airport construction - maintenance and cleaning of runways - equipment for runways signals by day and by night - air traffic control and assistance - electronic and radioelectric equipment - aerostation equipment - equipment for commercial assistance at the airports - equipment for hangars and aircraft maintenance - equipment for airport technical assistance - first aid equipment - equipment for board supplies - equipment for general airport services.

As always a number of congresses and conferences will be held during the days of the Show, representing a particular attraction for scientists and experts from all over the world.